

COMMERCIAL DESIGN STANDARDS

City of Kennewick, WA



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Scope and Applicability

These standards are applicable to all new and remodeled commercial developments in the City's commercial zoning districts when the cost of remodeling and expansion is equal to or exceeds 50% of the current assessed value of the structure as determined by the City's building official. General Standards are applicable to all commercial districts except for Downtown area. Corridor specific standards will be applicable to the development along the street corridors as shown on the map in this document. Commercial areas that are not included in the specific standards table shall follow the General Standards. Standards for mixed-use developments would apply to residential uses as well when located on the upper floors with commercial uses on the ground.

The standards are grouped into mandatory and recommended sections. They are available through the Kennewick Municipal Code. The mandatory standards shall take precedence in case there is a conflict with the KMC.

These standards are not intended to prohibit creative design and development solutions by professional designers/developers that might generate a better quality development. Accordingly, certain departures from the design standards may be permitted. In order for a departure from the mandatory standards to be allowed, the development proposal must demonstrate that the departure would result in a development that better meets the intent, objectives and design principles of the design standards. The Director of the Community Planning Department shall have the final authority to resolve any conflicts in the standards in order to protect the City's objectives and goals.

All mandatory standards are to be followed throughout the commercial development. Conformance will be reviewed during the permitting process. Although a developer is not bound to follow all of the recommended standards, these allow more flexibility than the existing code and promote better aesthetics and design solutions.

Objectives and Design Principles

Objectives:

- Promote economic vitality
- Promote good business environment and destination marketing
- Provide simplicity and clarity of the regulations
- Promote quality and creativity in design
- Offer flexibility and options

Design Principles:

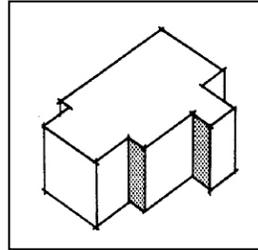
- Safety and vitality - livable pedestrian friendly community
- Compatibility - respect the positive attributes of Kennewick
- Sustainability - preservation of natural resources
- Efficiency in the delivery of public infrastructure
- Variety and creativity in design

Definitions

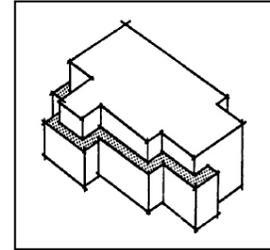
Articulation and Modulation

Articulation: Shifts in the plane of walls, setbacks, stepbacks, overhangs, and details in order to create variation in a building façade and divide large buildings into smaller identifiable pieces.

Modulation: Variation in the building mass through the use of stepbacks, setbacks, diminishing upper floors areas, and/or projecting roof overhangs.



Vertical articulation.



Horizontal articulation.

Big-box

Large, stand-alone retail stores generally over 100,000 square feet drawing customers from a large area. Typical big-box retails are:

- Auto oriented
- Franchises
- Typical appearance is boxy with solid walls, no openings, and massive
- Usually single-story structure but 3 stories tall.

Blank Walls

Walls without windows, plantings or architectural elements, such as modulation features.

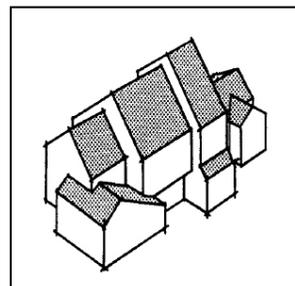


Façade

Façade means the exterior wall of a building exposed to public view or that wall viewed by persons not within the building (KMC 18.130.660).

Massing, Building Mass

Building mass refers to height, width and depth of a building structure. Massing means grouping of three-dimensional building forms to achieve variation.



Multi-planed façade and roof with desired articulation and massing.

Mixed Use, Horizontal

Development that incorporates residential and pedestrian-oriented commercial uses (and any additional uses allowed in the underlying zoning district) on the same lot or contiguous lots in the same zone. This type of development may occur within a single building where uses are separated by walls or within multiple buildings.

Mixed Use, Vertical

Development that incorporates residential and pedestrian-oriented commercial uses (and any uses allowed in the underlying zoning district) within the same zone. This type of development may occur within a single building or multiple buildings on the same site or contiguous sites where uses are separated by floors.



Vertical mixed-use

Outdoor Display Area

Areas outside the building structure where merchandises are placed for display purposes for customers.

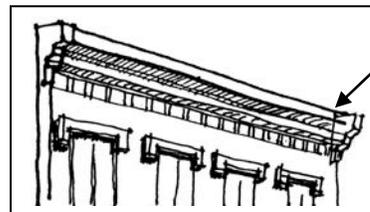
Outdoor Storage Area

Outdoor areas where merchandise is kept for temporary storage purposes but not for display; or, the keeping in an unroofed area, of any goods, junk, material, merchandise, or vehicles in the same place for more than 24 hours (KMC 18.130.1450).

Parapet and Cornice

Parapet is the vertical extension of the main walls of a building above the roofline.

Cornice is the horizontal projection, molded or otherwise decorated that crowns the top of the building.



Projected cornice.

Pedestrian-Oriented Commercial Use

A commercial enterprise whose customers may likely arrive by foot and may include restaurants, retail shops, personal service businesses, travel services, banks (except drive-through windows), and other similar establishments that generally benefit from pedestrian activity.

Proportion

The ratio of building elements, their height, mass and depth. Good proportion is a harmonious arrangement or relation of parts or elements within a whole.

Qualified Professional

A person with experience and training in the pertinent discipline. A qualified professional must have a B.S. or B.A. or equivalent degree in planning, design, engineering, landscaping or similar fields.

Roofline

The outer edge of the roof that provides visual terminus to the tops of buildings.



Roofline Variation

The roofline articulated through a variation or step in roof height or detail, such as: pitched roof, projecting cornice, articulated parapet, or terraced roof.

Scale

The relationships of a development and/or its elements in terms of size, height, bulk, intensity, and aesthetics, to one another and the surroundings. Human scale would identify the relationship of building with the human being.

SITE DESIGN STANDARDS

Compatibility with the Surrounding

Intent:

To ensure that businesses best reflect the consistency and positive character of the area.

Standards:

Recommended

1. Landscaping, building height, building location and orientation, massing, and architectural style should reflect local characteristics. They should be consistent with the positive attributes of the adjacent businesses and neighborhoods. Corridor specific design standards can be used to establish the positive attributes of an area.
2. Physical and visual connection between businesses should be considered during the design phase.
3. Site and building design should preserve important view corridors throughout the City. Height of the buildings should graduate with the natural slope in order to prevent obstruction of view corridors.
4. Buildings should offer view of Columbia River and the river basin to the north and east, and the mountains to the west and south whenever applicable. Visual and physical access to parks and open spaces should be provided whenever applicable.

DO



Consistency with slope and surrounding developments in Quinault area.



Compatibility in terms of similar roof-form.



View of the river basin.

Building Location and Orientation

Intent:

To enforce the physical and visual connection between businesses, streets, and open spaces in order to create a vibrant and attractive business and streetscape environment.

Standards:

Mandatory

1. Setback. Buildings shall be located closer to the street to enhance the streetscape character.
 - a) Corner sites: Buildings for the corner sites must be maximum 5 ft from the back of the walk. When the building abuts the sidewalk, or the setback is zero (0 ft), the sidewalk shall be at least 10 ft wide. Such setback shall not be required when the corner is designed as public open space, plaza, landscaping and/or prominent massing.
 - b) Other sites: According to the streetscape options mentioned in the “Specific Standards” section (page 26).
 - c) Setback standards can be altered for sites with steep slopes or other critical areas, or for a specific design approved by the Planning Director that meets the intent, objectives and principles of the design standards.
2. Solid, blank façades and service areas shall be oriented away from public streets.

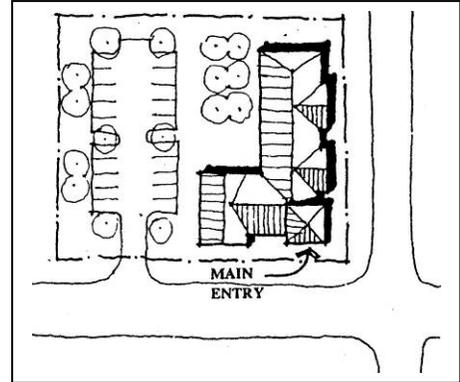
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DO NOT



Solid façade facing the street.

DO



Building close to the street.



Building facing the street and close to the street.



Building abutting wider sidewalk.

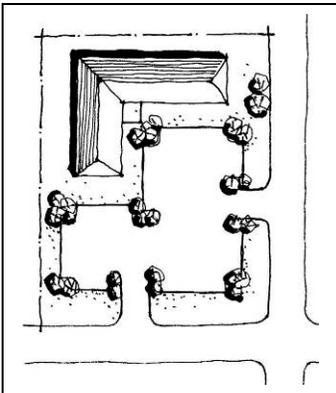
Building Location and Orientation (contd.)

3. Buildings shall be oriented to face public street, open space or plaza.
4. Façade facing public streets or public spaces shall have physical or visual connection with such streets or spaces by having a combination of prominent primary or secondary entrance, display windows, and transparent façade.

Recommended

5. Buildings orientation should be considered to prevent the strongest summer sunlight and glare from reflecting surfaces during the peak use hours.

DO NOT



Building away from the street, facing the parking lot.



Solid façade facing the street.

DO



Building facing the landscaping and outdoor seating area.



Do - Building close to the street with windows.

Do Not – No street trees and inadequate width of the sidewalk.

Access

Intent:

To provide safe and efficient access to the site and the building for both vehicular and pedestrian traffic.

Standards:

Mandatory

1. Both vehicular and pedestrian accesses to the site must be visible, and easily accessible from the street.
2. Driveways shall be shared and consolidated in order to reduce the number of curb cuts.
3. Pedestrian access and walkways shall be provided from the street and parking lot to the building (See "Pedestrian Walkways" for details; pg. 7).
4. Vehicular access shall be away from major intersections according to the guidelines in the Kennewick Administrative Code. All driveway standards such as width, throat distance, easement, access location etc. shall be according to the KMC and KAC.

Recommended

5. Special paving for the access to the site, and entrance to the building from pedestrian walkways are encouraged as long as they comply with ADA standards.

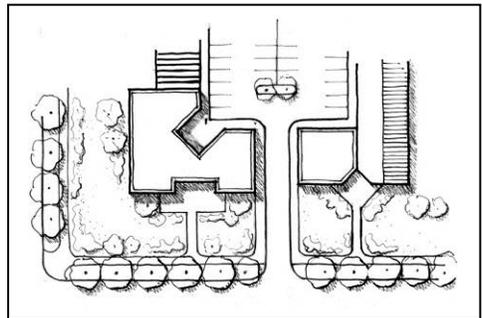
DO



Businesses

Shared parking lot

Shared access and parking between multiple businesses.



Pedestrian access and shared vehicular access driveway.

DO NOT



Too many curb cuts on a major street.



Special paving at the building entrance area

Parking

Intent:

To minimize the impact of parking facilities by treating them in such a way that do not dominate the main structures and the pedestrian realm and maintain the safety and viability of the businesses.

Standards:

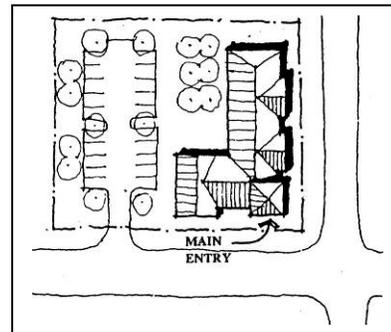
Mandatory

1. Projects with 20 or more parking stalls (SEPA threshold) must use **one** of the following:
 - a) At least 70% of the total parking shall be located behind or to the side of the building.
 - b) When one double-aisle parking is located in the front between the building and the street, the amount of required interior landscaping¹ and the number of trees shall be increased by 50% of the existing code requirements, and the setback of the parking lot from the street right-of-way shall be 10 ft.
 - c) When there is more than one double-aisle of parking in the front between the building and the street, the amount of required interior landscaping and the number of trees shall be increased by 50% and the setback of the parking lot from the street right-of-way shall be 15 ft.
2. The parking lots must be landscaped with large canopy type trees as listed in the recommended tree listing or as approved by the City, that provide shade without obstructing visual access to the businesses, and allowed to mature and be maintained a minimum height of 30 ft. A tree must be within 75 feet of any parking stall..
3. The parking lots must be visible from the building and adequately lit in order to ensure safety.
4. Large parking lots shall be broken into smaller ones with landscaping and pedestrian connections. A landscaped area shall be within 75 ft of any parking stall (KMC 18.64.060(4)(e)).

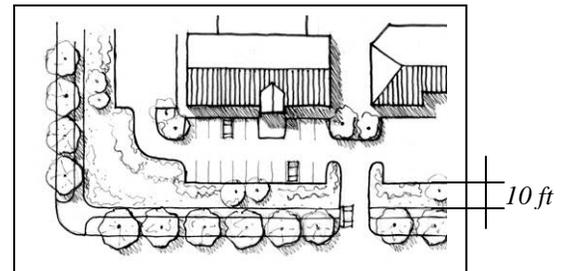
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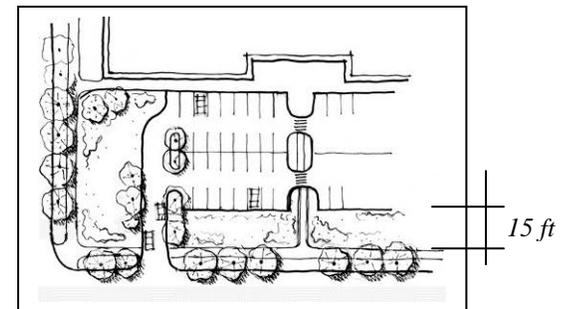
Parking behind the building.



1.a) Parking on the side of the building.



1.b) One double aisle of parking.



¹ Interior landscaping does not include landscaping in the setback areas

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Parking (contd.)

- 5. When parking is shared among multiple businesses, the total combined required parking shall be reduced by 10% (KMC 18.72.080(3)).
- 6. Safe pedestrian walkways shall be provided from the parking lot to the building and public sidewalks (See “Pedestrian Walkways” for details, pg. 7).

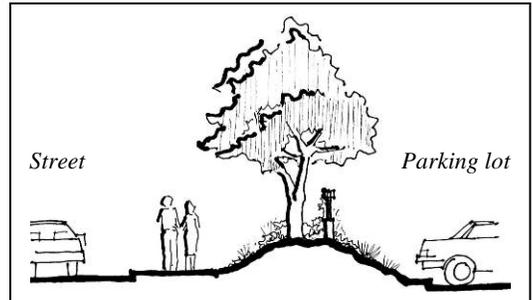
Recommended

- 7. Trees in the parking lot should be planted in groups or distributed in a manner that is consistent with the building design and maximizes the shading area throughout the parking lot.
- 8. When parking is located in the front, low walls, berms and planters (maximum 3 ft high) should be used to make the parking area less visible from the street. When the parking lot and the street are at different grades, retaining walls and trees should be utilized to make parking less visible from the street.
- 9. Landscaped medians are encouraged between face-to-face parking bays.
- 10. A 15% off-street parking reduction may be permitted for parking areas greater than 16,000 sq-ft (KMC 18.72.080).
- 11. Parking lots that utilize trees in excess of those required will be allowed to reduce the total square footage of the required landscaping according to KMC 18.64.060(4)(g).

DO

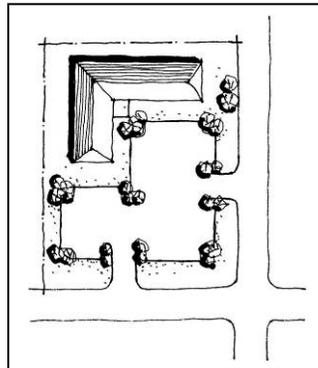


Landscaping in the parking lot with pedestrian walkways.



Parking lot screened with berm, low wall and landscaping.

DO NOT



DO NOT



Parking in the front.



Parking in the front.

Walkways, Pedestrian Connections

Intent:

To increase connectivity through the network of pedestrian and vehicular circulation, and promote a pedestrian friendly environment for businesses.

Standards:

Mandatory

The following standards are mandatory for projects with 20 or more parking stalls (SEPA threshold), and recommended for projects with less than 20 parking stalls.

1. Walkways shall be provided to establish pedestrian connections between parking lots, sidewalks, open spaces, parks and plazas, buildings, adjacent businesses and neighborhoods.
2. Minimum width of the walkways shall be 5 ft or according to the ADA regulations.
3. Walkways shall be located in safe walking environments, using buffers from busy streets, adequate lighting and visibility, connections with businesses, and separation from the vehicular circulation.
4. Striping may be used to delineate walkways from the parking lot to the building. All other pedestrian connections and walkways shall use a combination of special paving material, such as brick paving, stone blocks, colored or stamped concrete, standard concrete or similar material; or other approved methods that physically delineates the walkway.

Recommended

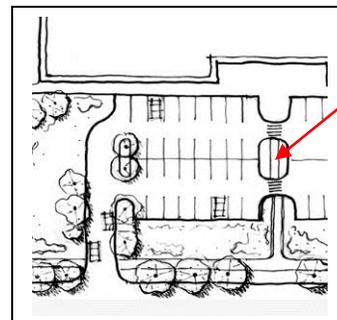
5. Weather protection, such as covered walkways with canopies should be provided in front of the building.
6. Walkways should be connected with parks, open spaces and plazas whenever appropriate.

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DO



Physically delineated walkway.



Walkway connecting the sidewalk and parking lot with the building.

Walkways, Pedestrian Connections (contd.)

7. Pedestrian walkways should be a combination of hard surface, living ground cover (such as grass, shrubs), and trees. Other landscaping material should include pedestrian scale lighting, bollards, trellises, and special interest landscaping.
8. Pedestrian connections should include steps or stairways when there is a significant level difference.

DO



Pedestrian walkways from the parking lot with special interest landscaping and paving.

DO



Pedestrian connection with steps from the business to the transit stop.



Pedestrian connection from the parking lot

DO NOT



No distinct pedestrian connection

Treatment of the Corner

Intent:

To ensure that businesses at street intersections can serve as landmarks and be integrated with the pedestrian realm.

Standards:

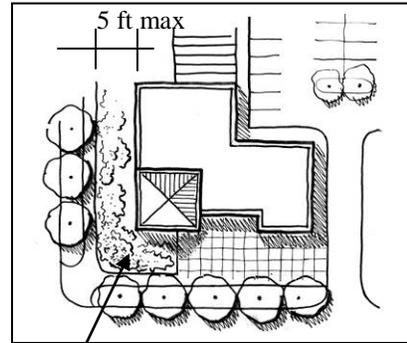
Mandatory

1. For sites at street intersections, **one** of the following must be done to treat the corner:
 - a) The building must be located closer to the street according to the “building location and orientation standards” for corner sites (pg. 2) and comply with the standard no. 1 in the “treatments of the corner” in the building design section (see pg. 22).
 - b) The corner of the site must be treated with a plaza, landscaping and/or artwork and must be visible to public.
2. Corner treatments must comply with the vision triangle requirements (KMC 18.66.060).

DO



Building close to the street at a corner site with artwork



Landscaping

DO



Building close to the street.



Plaza at the corner of the site.



Building close to the street with prominent massing.

Outdoor Common Areas, Open Spaces

Intent:

To enhance the vitality of the business environment by providing pedestrian-friendly, accessible, usable, and visually connected open spaces within private developments.

Standards:

Mandatory

1. When businesses offer outdoor common areas, they must be visible and accessible at least to the users of the businesses.

Recommended

2. Each business should offer outdoor common areas or open spaces. Open space may be provided in the form of plazas, courtyards, extended wider sidewalks with seating, or open green space with landscaping.

Open or semi-open patios with canopies or pergolas opened outside as an extension of the building, with benches, seating, special paving, and shade trees, can also be considered as outdoor open spaces.

3. Open spaces around retail areas should be more informal, active and user-friendly. Open spaces in office areas could be more formal and less active but visually appealing.
4. Open spaces could include following elements:
 - Public art
 - Special paving
 - Special interest landscaping
 - Pedestrian scale lighting, bollards, and accent lighting
 - Seating such as benches, low walls
 - Water features
 - Pergolas
 - Trellises

DO



Extended wider sidewalk with sitting.



Formal open space in office areas.



User-friendly open space in retail areas.



Outdoor sitting in Marine Land Village.

Service Areas

Intent:

To minimize negative visual impacts of service areas from other businesses and adjacent areas.

Standards:

Mandatory

1. Loading and service areas shall be located in the least visible areas of the site and shall be screened from view by a fence (preferably of masonry or similar material), landscaping and trees or a combination of both. In cases where there are no other options available due to the street and site layouts, service areas and backside located near public streets must be screened from public view with solid screen landscaping, berms, grading and/or masonry or similar type of fencing. The use of chain link fencing is prohibited.
2. Trash dumpsters or collectors must be in an enclosed area and located in the least visible areas of the site. Enclosures should be complementary in design to the parent structure. The use of chain link fencing is prohibited.
3. Individual trash receptacles for public use do not need to be in an enclosed area, but must be covered and strategically located to accommodate the public.

Recommended

4. Service area design should be consistent with the overall architectural and site planning scheme.
5. The access to the enclosed trash receptacle areas should be indirect, and may include small-scale “man-gates”.

DO



Service areas on the backside of the building screened with landscaping and fence.



Trash and service area located behind and screened.

DO NOT



Open trash receptacles – no screening.



Enclosed trash receptacles.

Lighting

Intent:

To establish lighting compatible with the development as well as the surrounding area and streetscape. To promote pedestrian and vehicular safety, and to improve the aesthetic appearance of public and private streetlights.

Standards:

Mandatory

1. All lighting features must be shielded, equipped with refractors, or placed indirectly to prevent stray upward light or direct light causing glare in order to comply with the City’s Outdoor Lighting Ordinance (KMC Chapter 18.76).
2. Parking lot must have a minimum illumination of 0.6 foot-candles at the ground level.

Recommended

3. Pedestrian-scale lighting should be provided on the walkways, parking lots and open spaces that are consistent with the character of the development.
4. Special interest and accent lightings are encouraged to be integrated with landscaping and streetscape features.
5. Lighting incorporated with timer and motion detectors can be considered to increase efficiency of use and energy.
6. Indirect wall-mounted lighting with shielding is encouraged.
7. Lighting poles in the parking lot should be coordinated with the planting strips and can have low-height masonry base (18” to 20” max.) when they are in combination with landscaping.

DO



Shielded pedestrian scale streetlight on Washington Street.



Parking lot lighting coordinated with the planting strip.

DO



Indirect lighting.

Respect/Protect Natural Features

Intent:

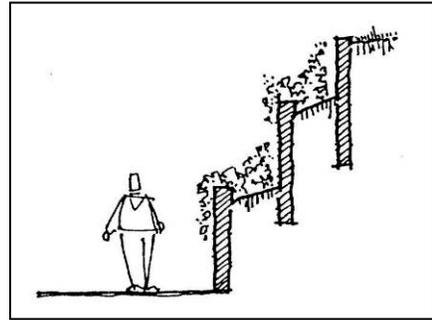
To preserve and maximize the use of natural features in the site development; to reduce adverse impact on natural features in the development.

Standards:

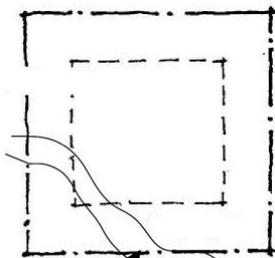
Recommended

1. The location of building should respond to site characteristics, and the surrounding area. (Topography, steep slopes, wetlands, views, existing vegetation and built-form).
2. For hillside developments on steep-slope sites - buildings should be stepped down the hillside to match the topography. Terraced retaining walls should be a maximum of 5 ft high.
3. Building layout can be modified to preserve natural features.

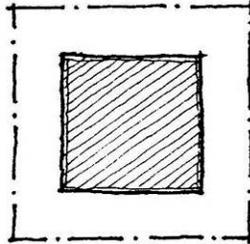
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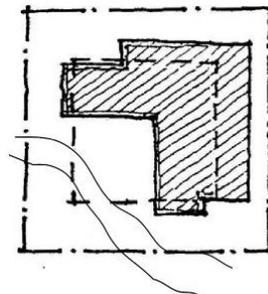
Terraced retaining wall 5 ft maximum height.



Existing creek on the site.



Buildable area.



Development respecting the site feature.

On-Site Tree Retention

Intent:

To preserve and promote natural growth of trees and other landscaping plants in order to create a sense of place, attract more customers, and improve the physical environment of the businesses.

Standards:

Mandatory

1. Trees, including street and landscaping trees must be allowed to mature and be maintained without obstructing the visual access to the storefront at a height of at least 30 ft, or a height that best reflects the building design, and business and storefront character. For example, taller buildings should plant trees that grow up to 40 to 50 ft or more upon maturity. Any alternative must be approved by the Director of the Community Planning.

Recommended

2. Existing mature trees should be preserved with their natural sizes and shapes. Pruning of trees is discouraged when it is not consistent with the approved site plan permit, approved landscape plan, overall site design scheme and specific tree characteristics.
3. Businesses should plant larger tree species according to the City's recommended tree listing.
4. Pruning off only lower branches is recommended as trees grow taller, in order to allow greater visibility of storefronts and provide shading.
5. Businesses should hire International Society of Arboriculture (ISA) certified arborists to prune large or small trees.
6. Building layout should be modified to preserve trees. In case it is impossible to preserve existing trees, new trees should be planted at the ratio of at least 2 new trees to replace one mature tree.

DO



Large trees properly pruned and maintained offer visibility for the businesses.

DO NOT



Pruning trees from the top branches.

Drive-thru Lanes

Intent:

To minimize adverse effects of vehicular traffic on the adjacent neighborhood and businesses.

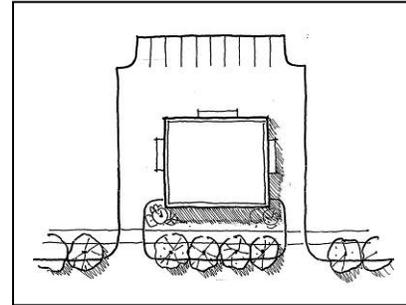
Standards:
Mandatory

1. The stacking lanes shall not be located between the public street and the building unless the lane is screened from view by landscaping screen, grade difference, or similar type of buffer.

Recommended

2. Stacking length should be buffered and screened from public view whenever appropriate.
3. Kiosk area between drive-through lanes in a teller should be landscaped without obstructing driver's vision.

DO



No stacking lane between the building and the street.

Fencing

Intent:

To offer quality, safety and transition between residential neighborhood and businesses.

Standards:
Mandatory

1. Barbwire and razor wire fences shall not be allowed unless in specific conditions when they are required for business safety and are not viewed from public street.
2. Fences facing the street shall not be allowed unless the business specifically requires such for:
 - a. Business safety which needs to be approved by the Planning Director;
 - b. Screening from residential;
 - c. Screening the parking lot from public view; in such cases fence shall be made of solid decorative concrete or masonry walls in combination with trellises or similar features.
3. Fencing shall be consistent with the overall building design.

Recommended

4. When businesses use fencing, open type fences with masonry posts, solid decorative concrete or masonry walls or other durable material is recommended.

DO



Screening from residential.

Transit

Intent:

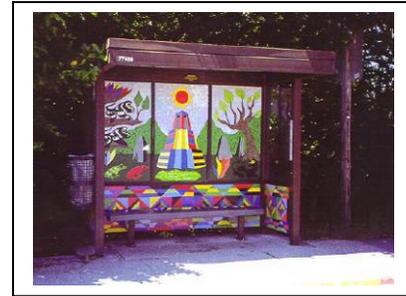
To promote the use and availability of the transit system and ensure safety at transit stops.

Standards:

Recommended

1. Developer/owner should consult with the City’s Traffic Engineer and Benton Franklin Transit Authority to determine the practicality of a bus stop near the development or how the site can be served by transit.
2. The required number of parking stalls may be reduced by 10% if the developer includes an improved, permanent on-site bus stop, provided the bus stop is on a transit route, and development is in accordance with the Benton Franklin Transit Authority’s requirements (KMC 18.72.090 (4)).
3. Convenient pedestrian connection with ADA accessibility should be provided.
4. Developer/owner should consult with the Benton Franklin Transit authority to include a bus stop design consistent with the development. Use of bright color and artwork are encouraged in the design
5. Developer/owner is encouraged to provide bus-stop structures to provide protection from rain and sun.

DO



Use of public art in transit stops.

BUILDING DESIGN STANDARDS

Massing, Architectural Features

Intent:

To reduce the bulk of the buildings by breaking down the mass into human scale; to offer variety and consistency along the street face.

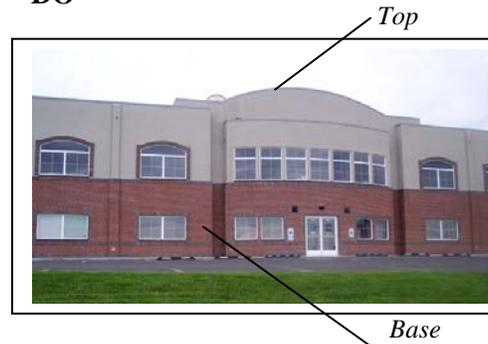
Standards:

Mandatory

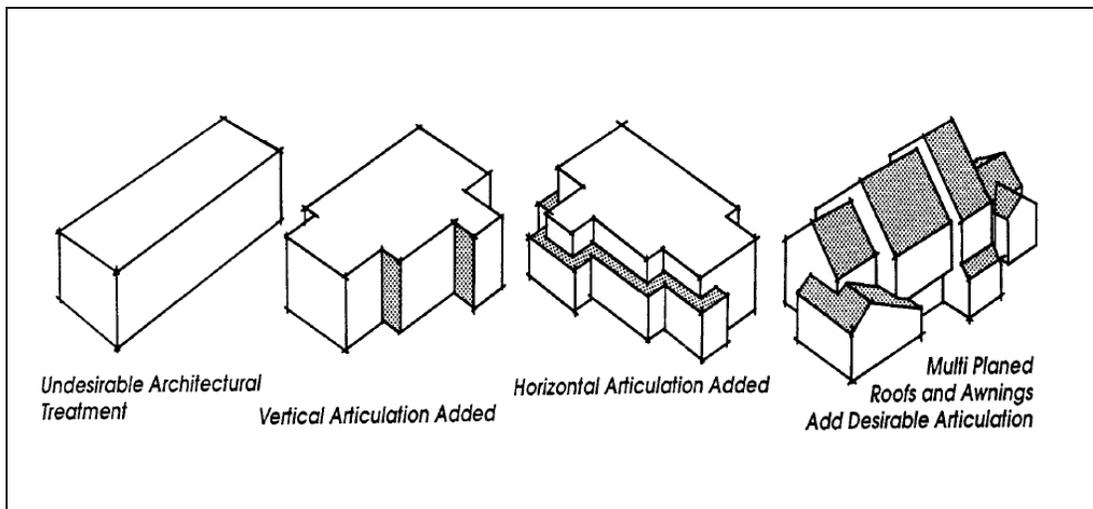
1. Multi-story buildings must display the proportion of a “Base”, “Middle” and “Top” in massing. “Top” can be expressed by using sloped, gabled or flat roofs. Flat roofs shall have cornices, parapets, or similar special features to act as the top of the building. Step-backs of the buildings at the upper floors can also be used as a method to express base, middle and top.
2. Street facing façade more than 50 ft long shall be treated with the following:
 - a) Change in the roof or wall plane (4 ft minimum)
 - b) Projecting or recessed elements
 - c) Varying rooflines at 4 ft minimum.

(Continued to the next page)

DO



Breaking down the mass



Massing, Architectural Features (contd.)

- 3. Solid blank facades must be avoided on the front or street facing sides of the building. They must be treated with windows, entrances, canopies, cornices, and by articulating the façade and/or screening with landscaping.
- 4. For slope roof structures, the slope of the roof must not be less than 4:12 except for a specific design to be approved by the Planning Director.

Recommended

- 5. Building height should be compatible with the surrounding building heights and character of the area.
- 6. Buildings should use elements such as canopies, awnings, trellises, pergolas, and arcades to provide human scale.
- 7. Solid blank facades when unavoidable should be treated with modulation, canopies, wall-mounted lighting, artwork, and landscaping trellises.

DO



Screening solid façade with landscaping.



Prominent Entrance

Intent:

To ensure visible, attractive, inviting, and efficient entrances to the buildings.

Standards:

Mandatory

1. Entrance to the building must be made visible and prominent by using large entry doors, porches, protruding, or recessed entrances.
2. Primary pedestrian entrances must face public streets, open spaces or plazas whenever available.
3. Light poles, signage and similar accessories shall be coordinated so that the view and accessibility to the entrance are not obstructed.

Recommended

4. Special paving at the entrance of the building is encouraged.
5. Special architectural treatment that signifies the entrance without destroying the architectural character is encouraged. Some examples are, using different material such as brick, stone, or glass at the entrance; or locating entrance at the conjunction of two building masses.
6. For multi-story buildings, the building entrance should be scaled down to relate to human scale.

DO NOT



No entrance on the public street side.

DO



Prominent building entrance - porch



Prominent entrance - recessed



Special paving at the building entrance area

Façade Transparency

Intent:

To create a visual connection between the indoor and outdoor environment in order to make businesses more attractive.

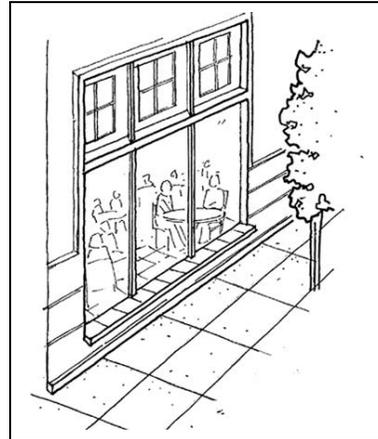
Standards:

Mandatory

1. Solid blank walls shall be avoided except for service areas where it shall be screened from public view (see Service Areas - pg. 11).
2. Façades facing public streets or public open spaces must be treated with a combination of prominent primary or secondary entrance, display windows, transparent façades, façade modulation, canopies, and/or awnings.
3. For retail buildings
Façade facing public streets or public open spaces or plaza shall have at least 40% of the façade area comprised of clear vision glass at the ground level unless a specific alternative design is approved by the Planning Director.
4. For office buildings
Façade facing public streets or public open spaces or plaza shall have at least 25% of the façade area comprised of clear vision glass unless a specific alternative design is approved by the Planning Director.
5. Clear vision glass for façade transparency at the ground level shall be calculated between 2 ft to 12 ft from the ground level.

(Continued to the next page)

DO



Indoor-outdoor relationship.



50% transparent façade area (between 2 ft to 12 ft from the ground level)

Façade Transparency (contd.)

6. Display windows along public streets shall express the type of business by displaying the products. Signage on the window shall not be considered as part of the display window.

Recommended

7. Tinted or reflecting glass is discouraged at the ground level when it prohibits visual connection between indoor and outdoor environments.

DO



Display window instead of solid blank wall.

DO NOT



Lack of adequate façade transparency fails to create an indoor-outdoor relationship.



Solid blank wall facing the street.

Treatment of the Corner

Intent:

To ensure that businesses at the street intersections can serve as landmarks and be integrated with the pedestrian realm.

Standards:

Mandatory

1. Buildings located at the corner of street intersections, shall have one of the following:
 - a) A primary pedestrian entrance complying with the standards of “prominent entrance” (see pg. 19), and accessible from the corner street sidewalks.
 - b) Distinctive massing and roof form of the building to mark the intersection as a landmark.
 - c) Other architectural features such as porches, canopies, and display windows at the corner.
2. Signage and accessory structures must not obstruct the view of the building at the corner.
3. Corner treatments must comply with vision triangle requirements (KMC 18.66.060).

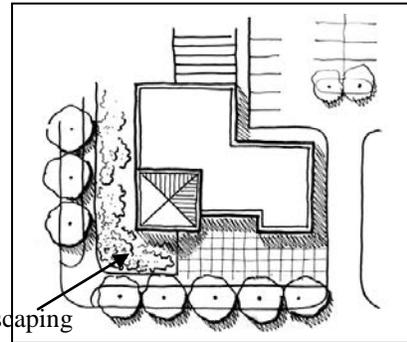
Recommended

4. Corner of the buildings should be coordinated with the overall site design scheme.

DO



Prominent roof-form and pedestrian entrance at the corner.



Landscaping, distinct roof-form and pedestrian walkways at the corner.

DO



Building close to the street with artwork.



Building close to the street at the corner

Backside of the Building

Intent:

To reduce the negative impacts of the backside and service areas.

Standards:

Mandatory

- 1. Loading and service areas shall be located in the less visible side of the site.
- 2. Backside of the building and service areas must not be located facing a major public street. In cases where there are no other options, the street and site layout, service areas and backside located facing public streets must be screened from public view with solid screen landscaping, grading and/or fencing.
- 3. Backside of the building shall be consistent with the front side of the building in terms of design style, building material and architectural features.

Recommended

- 4. Solid blank facades should be treated with façade modulation, canopies, lighting, artwork, and landscaping trellises.

DO



Backside of the building facing a public street is screened with trees and landscaping.



Backside of the building with secondary access, canopies and ornamental treatment reflecting the floral business.

DO NOT



No screening on the backside facing public street.

Screening of Electrical and Mechanical Equipments

Intent:

To minimize negative visual impacts of service equipment on the businesses and the adjacent areas.

Standards:

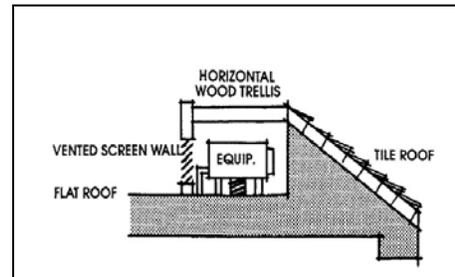
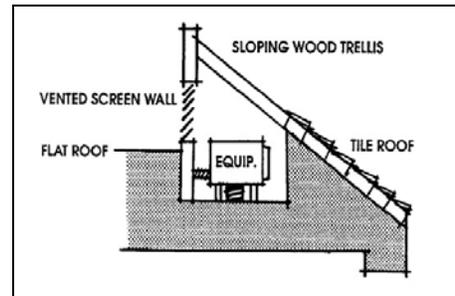
Mandatory

1. Electrical and mechanical equipments when placed on the rooftop shall be obscured from view (i.e. by using parapet).
2. Mechanical equipments when placed on the ground shall comply with the standards under "Service Areas" (see pg. 11).

Recommended

3. Increased parapet height can be accepted when used for screening the mechanical equipment from view.
4. Screening of electrical and mechanical equipments shall be consistent with the overall building design style, building material and architectural features.

DO



Screening rooftop equipments.

Material

Intent:

To enhance the character of the building and the area in order to make the business more attractive to customers.

Standards:

Recommended

1. Exterior building material should be selected for suitability and high degree of performance. Building façade should be a combination of material to add variation in the façade design. Material used can be masonry, stone, stucco, wood or similar material. Metal building exteriors are discouraged unless the specific design is approved by the Planning Director.
2. A combination of material should be used to add variation in the façade design.

DO



Combination of material adds variety.

STREETSCAPE STANDARDS

Streetscape

Intent:

To increase vitality of the street environment and integrate businesses with the street. To reduce the impact of heavy traffic and offer safe vehicular and pedestrian environment for businesses.

Standards:

Mandatory for All Streets

1. Plant material shall be a mixture of drought tolerant deciduous and evergreen trees. Drought tolerant plant materials may contribute up to 50% of the required landscaping ground cover and street trees. Selection of trees and plant materials shall be chosen from the City's recommended planting list or approved by the Planning Director.
2. Street trees must meet standards contained in KMC 18.21 and must be allowed to mature and be maintained without obstructing the visual access to the storefront at a height of at least 30 ft, or taller that best reflects the building design and business and storefront character.
3. Street trees shall be planted at least every 40 linear feet or be planted in groups in the planting strip. Street tree locations must be coordinated with the street lighting and utility plans to minimize interference. A detailed landscape plan designed and signed by a licensed Landscape Architect or a similar qualified professional must be submitted for approval by the Planning Director.
4. All landscaping elements, plant materials and street trees shall be planted or installed by the developer and maintained by the property owners or business owners. A maintenance agreement among adjoining property owners shall be required; the agreement shall run with the land.
5. A separate irrigation system designed for the health of the street trees on arterial streets (e.g. bubbler system) maintained by the owner shall be required.

(Continued to the next page)

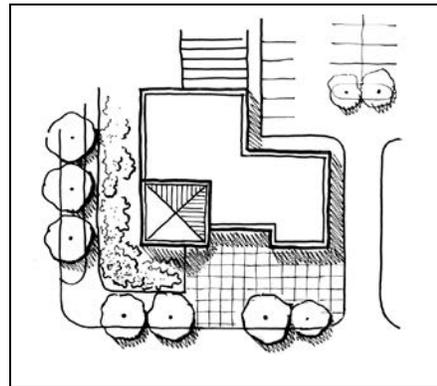
DO



Large street trees.



Special pavement, sitting, and trellises on wider sidewalk.



Streetscape coordinated with the business frontage.

Streetscape (contd.)

Mandatory Option A

- 6. Minimum 7 ft wide planting strip between the road and the sidewalk
- 7. Sidewalk width shall be 7 ft minimum.

Mandatory Option B

- 8. Sidewalk must abut the road.
- 9. Sidewalk width shall be 10 ft minimum with at least 5 ft clear for thorough traffic (i.e. maximum 5 ft wide planting wells, and locations for street lights, benches and other street furnishings within this 5 ft, leaving the rest 5 ft clear for pedestrian).

Recommended

- 10. For Option-A, landscaping strip can be a mixture of hardscape and living plant material with hardscape consisting maximum 50% of the total planting strip area.
- 11. Planting strips are encouraged to be incorporated with the overall storm water drainage plan.
- 12. Incorporation of artwork with the hardscape is encouraged.

DO NOT



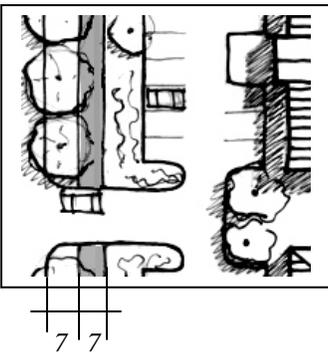
Narrow sidewalk and no landscaping.



No street tree.

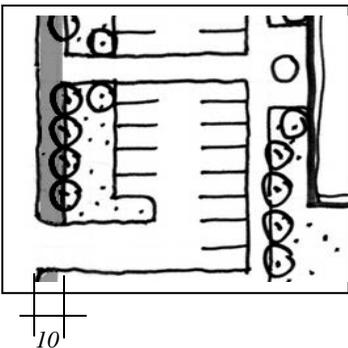
DO

OPTION A



Option A - Planting strip between sidewalk and the street.

OPTION B



Option B - Wider sidewalk with streetscape.

CORRIDOR SPECIFIC STANDARDS

Corridor Specific Standards

Following standards are mandatory when they correspond to the mandatory standards in the general design standards section of this document. Other standards are strongly recommended. A map indicating different corridors is included at the end.

Category	Roads/ Areas	Specific Standards that are unique to the corridor
Highest pedestrian emphasis (Streetscape predominantly option B)	Washington St. (1 st Ave. to Clover Island Inn)	<ul style="list-style-type: none"> • Building location and orientation: building close to the street. • Parking: 70% or more parking behind or side (General standards 1(a)). • Treatment of the corner (for corner sites) – plaza and building at the corner with prominent entrance. • Streetscape – Option B
	Gage Blvd.	<ul style="list-style-type: none"> • Building location and orientation: building close to the street. • Parking: Maximum one double-loaded parking in the front (General standards 1(a) or 1(b)). • Massing and architectural features: Prominent entrance. • Streetscape – Option B
	Grandridge Blvd.	<ul style="list-style-type: none"> • Building location and orientation: building close to the street. • Parking: 70% or more parking to be located behind or side (General standards 1(a)). • Treatment of the corner (for corner sites) – landscaping and plaza. • Streetscape – Option A or B
	W. Quinault Ave. Center Parkway (between Grandridge and Gage)	<ul style="list-style-type: none"> • Building location and orientation: buildings can be set further back to respect the slope. • Parking: 70% or more parking to be located behind or side (General standards 1(a)). • Pedestrian Connections: Between businesses, parking lot to building; cross street pedestrian connection. • Respect/Protect Natural Features: Respect the slope – terraced or wider setback; preserve view corridors. • Streetscape as appropriate respecting the slope, combination of Options A and B.
	Okanogan (between CCB to Rio Grande) Rio Grande (from Grandridge to Kellogg) N. Young (Deschutes to Okanogan) 4 th (between Olympia and Garfield)	<ul style="list-style-type: none"> • Parking: Maximum one double-loaded parking in the front (General standards 1(a) or 1(b)). • Buildings close to the street. • Prominent Entrance: Prominent building entrances facing the street. • Treatment of the corner (for corner sites): Treat with plaza or landscaping; building at the corner with prominent entrance. • Massing: Avoid solid blank façade on public street side. • Streetscape – Option A or B.
Medium pedestrian emphasis (streetscape option A or B)	Kennewick Ave. (Between Union and Olympia)	<ul style="list-style-type: none"> • Massing: Avoid solid blank façade facing any public street. • Treatment of the corner (for corner sites): Treat with plaza; building at the corner with prominent entrance. • Streetscape: Option B; street tree height 30 ft or more.

CORRIDOR SPECIFIC STANDARDS

Medium pedestrian emphasis (streetscape option A or B)	Washington St. (between 1 st and 10th)	<ul style="list-style-type: none"> • Streetscape – Option A or B
	Canal Drive (from Olympia to the east)	<ul style="list-style-type: none"> • Streetscape – Option B
	Kellogg St. (from Canal to Clearwater) Edison St. (from Canal to 1st) 10 th (between W. Clearwater and CCB)	<ul style="list-style-type: none"> • Building location and orientation: Buildings close to the street and facing the street. • Access: Shared and consolidated driveways. • Parking: Maximum one double-loaded parking in the front (General standards 1(a) or 1(b)); shared parking lot. • Pedestrian Connections: Between businesses; cross street pedestrian connection. • Streetscape – Option A or B; street tree height 30 ft or more.
	Clearwater (from Edison to Hwy 395) Vista Way	<ul style="list-style-type: none"> • Building location and orientation: Buildings close to the street and facing the street. • Access: Shared and consolidated driveways/ reduced curb-cut • Parking: Maximum one double-loaded parking in the front (General standards 1(a) or 1(b)); shared parking lot. Parking lot landscaping. • Pedestrian Connections: Between businesses; cross street pedestrian connection. • Treatment of the corner (for corner sites): Treat with landscaped plaza; building at the corner with prominent entrance. • Streetscape – Option B; street tree height 30 ft or more
Medium-low pedestrian emphasis on heavy traffic areas (streetscape option A)	Clearwater (from Edison to the west)	<ul style="list-style-type: none"> • Building location and orientation: Buildings don't have to be close to the street, but adequate landscaping is required between the street and the parking lot or building. • Access: Shared and consolidated driveways/ reduced curb-cut • Pedestrian Connections: Between businesses, parking lot to building; cross street pedestrian connection. • Parking: Shared parking lot. Parking lot landscaping. • Streetscape: Option A or B; street tree height minimum 30 ft upon maturity.
	Columbia Center Blvd.	<ul style="list-style-type: none"> • Building location and orientation: Small buildings to the front, close to the street. • Pedestrian Connections: Safe pedestrian network; cross street pedestrian connections. • Parking: Shared parking lot, minimize total # of parking. Parking lot landscaping. • On-site tree retention. • Screening electrical and mechanical equipments. <p>Streetscape: Special street tree feature to buffer view of the parking lot from the street; shading canopy trees in the landscaped median.</p>
	Canal Drive (between CCB and Volland)	<ul style="list-style-type: none"> • Streetscape: Option A.
	Center Parkway (from Gage to the north extension)	<ul style="list-style-type: none"> • Treatment of the corner (for corner sites): Treat with plaza or landscaping; building at the corner with prominent entrance. • Streetscape: Option A. Street tree standards

CORRIDOR SPECIFIC STANDARDS

	<p>Step toe St. (existing and future extension)</p>	<ul style="list-style-type: none"> • Buildings don't have to be close to the street, but adequate landscaping is required between the street and the parking or building • Streetscape: Option A with planting strip.
<p>Heavy traffic areas and State Highways (streetscape option A)</p>	<p>Hwy 395</p>	<ul style="list-style-type: none"> • Access: Shared and consolidated driveways/ reduced curb-cut • Streetscape: Option A with wider planting strip between sidewalk and the road; special tree requirements to create buffer between walkways and the street; landscaped median. • Pedestrian Connections: Between businesses, parking lot to building; safe cross street pedestrian connection. • Limiting pedestrians and bi-cyclists.
<p>Gateways and Major intersections to emphasize.</p>	<p>BB-RR area - Columbia Dr. at the Blue Bridge and Columbia Dr. and Gum intersections. 10th (@ Hwy 395 intersection). 27th and Hwy 395 (from Union to Ely). 36th and Hwy 395 SR 397 and 10th Ave - near Fairgrounds Clearwater and I-82 (in coordination with Benton County and Richland) Clearwater and CCB Step toe and Gage (in coordination with Richland) Center Parkway and Gage CCB and Canal Edison and Canal</p>	<ul style="list-style-type: none"> • Treatment of the corner (for corner sites): Treat with landscaped plaza; building at the corner with prominent entrance. • Building location and orientation: Buildings front facing the public street, close to the street. • Special architectural styles, massing. • Parking: Shared parking lot, minimize total number of parking. Parking lot landscaping. Parking behind or side (General standards 1(a)).

Map

CORRIDOR SPECIFIC DESIGN STANDARDS

CITY OF KENNEWICK, WA
STREET MAP

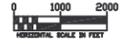


MAP LEGEND

- Highest Pedestrian Emphasis
- Medium Pedestrian Emphasis
- Medium-low Pedestrian Emphasis
- Heavy Traffic and State Hwys
- Downtown*
- BB-RR Area*
- Gateways and Major Intersections
- STREETS
- RIVER
- KENNEWICK
- URBAN GROWTH AREA

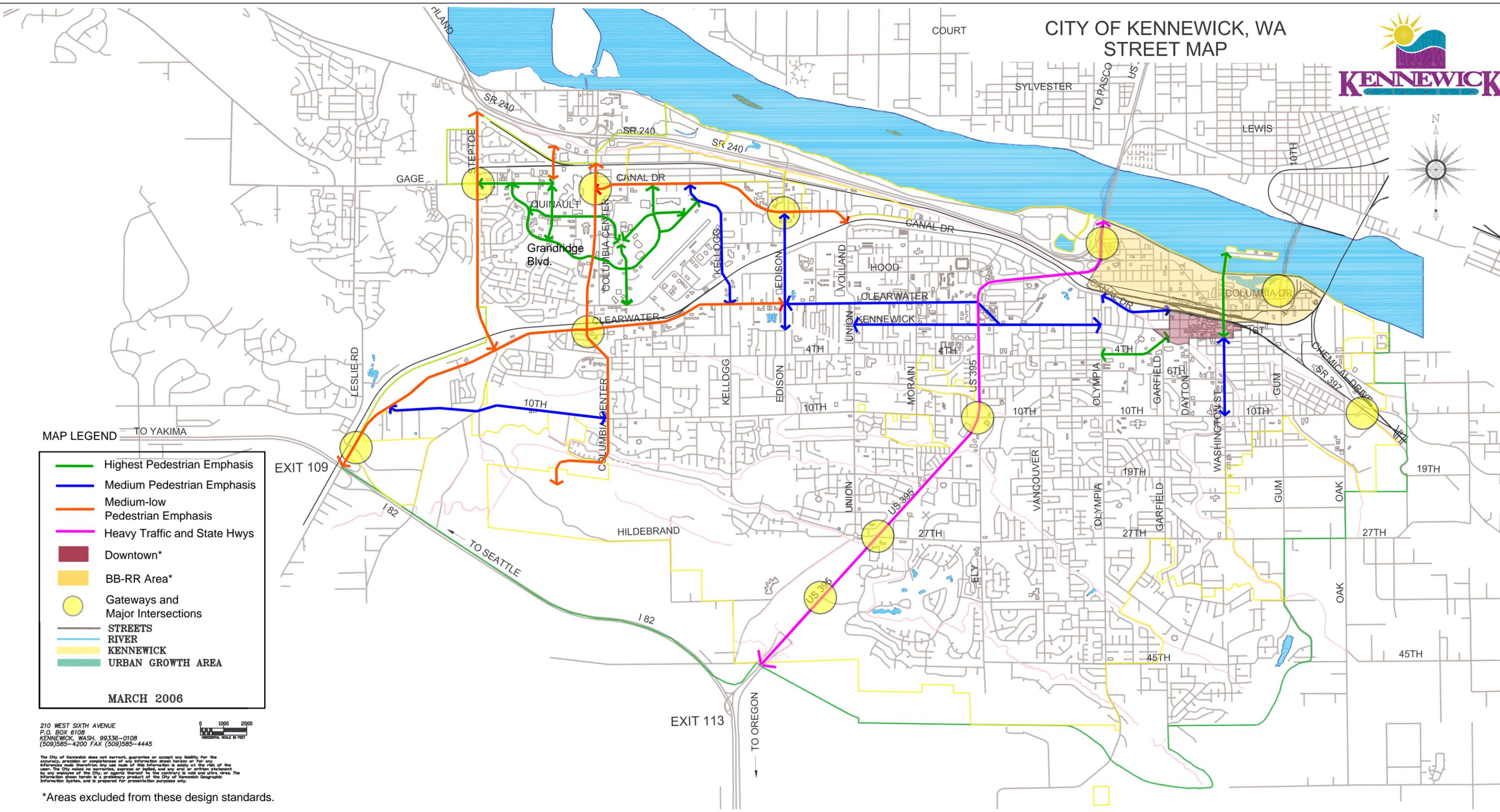
MARCH 2006

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*Areas excluded from these design standards.



BIG-BOX DEVELOPMENT STANDARDS

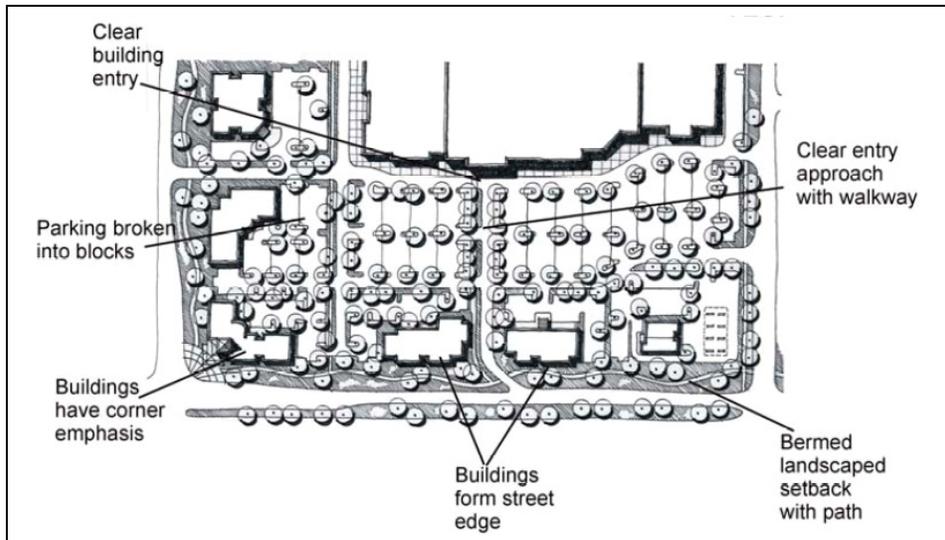
Additional Standards for Big-Box Developments

All mandatory standards in the general site and building design sections shall be applicable to big-box developments. The following standards are in addition to those general standards in order to meet the intent below. These standards are strongly recommended.

Overall Intent

To mitigate the negative impacts of the big-box businesses by improving their physical conditions and aesthetics. To create a livable environment and improve the relationship to the surrounding community.

Site Design Standards



Sketch source: City of Overland Park, Kansas

General Site Layout and Building Locations

Intent:

To reduce the impact of the large scale of big-box structures from the street side.

Standards:

1. Parent big-box structures should be located behind, and away from the street.
2. Smaller structures should be closer to the street.

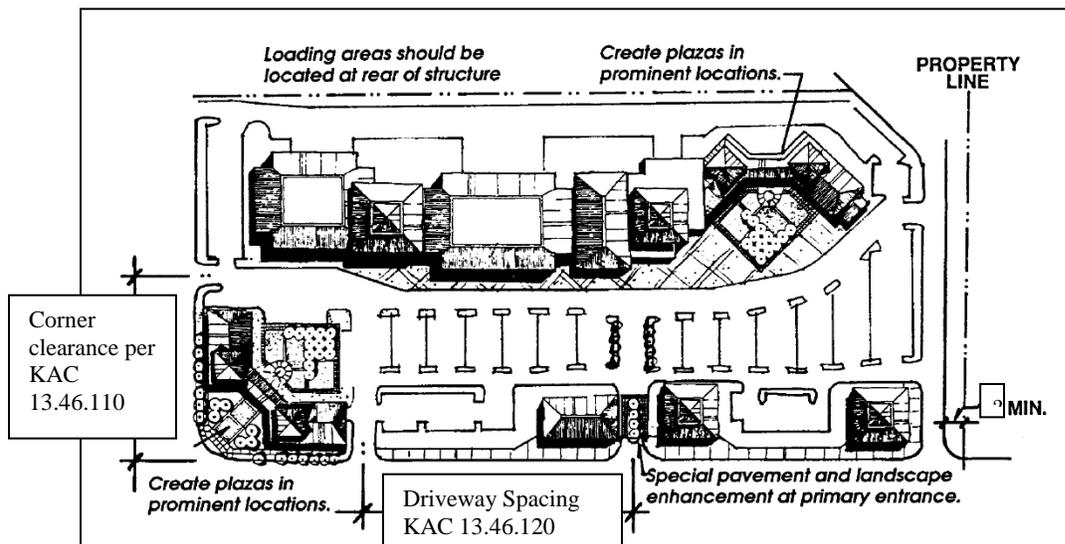
Parking Lot Design

Intent:

To offer safe pedestrian movement to and from the parking lot, to add human scale to the parking lot, to improve the physical and aesthetic integration between the parking lot and the building.

Standards:

1. Location. Between the smaller structures and the big-box.
2. Break down the parking lot into smaller sections by adding landscaping and pedestrian walkways, placing the buildings in between, following the site topography and similar techniques.
3. Clear directional signage for ingress-egress and parking shall be provided.
4. Reduced number of parking stalls is encouraged.
 - a) The number of parking required can be lowered by 15% for parking areas greater than 16,000 sq-ft (KMC 18.72.080).
 - b) If adjoining facilities can be developed and designed efficiently and economically as one facility, and the total parking area will be 10,000 square feet or more the total combined required parking stalls may be reduced by 10 percent (KMC 18.72.090(3)).
 - c) The required number of parking can also be reduced by 10% if the developer includes an improved, permanent on-site bus stop, provided the bus stop is on a transit route, and development is in accordance with the Benton Franklin Transit Authority (KMC 18.72.090 (4)).
 - d) While business can reduce the total number of the parking stalls, they are allowed to preserve some unpaved areas for overflow parking. Such areas can be treated with grasscrete, gravel, or landscaping with a combination of artwork, plazas, or playgrounds that later could be used for parking during the busiest times of the season.
 - e) Parking lots larger than the minimum required in the Code should have a 10% increase in the total landscaping area, number of trees, shading and pedestrian walkways in addition to other design standard requirements.



Overall Landscaping

Intent:

To soften the built form and improve the environmental quality of the area; to integrate the entire site with landscaping.

Standards:

1. Establish landscape buffers from residential uses.
2. Landscaping, trees, and trellises should be used to screen solid walls.
3. Special landscaping features for entryways, walkways, plazas and open spaces should also be used.

DO



Outdoor Storage and Display Areas

Intent:

To reduce the negative impact of these service areas.

Standards:

1. All non-enclosed outdoor storage areas must be located at the rear, or **non-street side** of the building.
2. Non-enclosed areas for seasonal sales and displays must comply with the temporary and parking lot businesses section of KMC (18.75.430).
3. If such areas need to be covered, the covering should be consistent with the overall building design.

Signage

Intent:

To ensure that signage becomes a part of the entire site and building design scheme, and reflects the community character in a positive way.

Standards:

1. Signage design in terms of size, type, color should be consistent with the architectural character of the building.
2. Signage should not be located to obstruct the view of the building frontage or the primary pedestrian entrances.

Building Design Standards

Entrance

Intent:

To ensure that building entrances are clearly visible and identifiable to pedestrians.

Standards:

1. The principal entry to the building should be made prominent with canopies, overhangs, protruding or recessed mass.
2. The building entrance should offer some degree of weather protection and act as a transition between indoor and outdoor environments.

DO



*Wal-Mart Store, Sequim, WA
ARCHITECT: Perry L. Butcher and Associates*

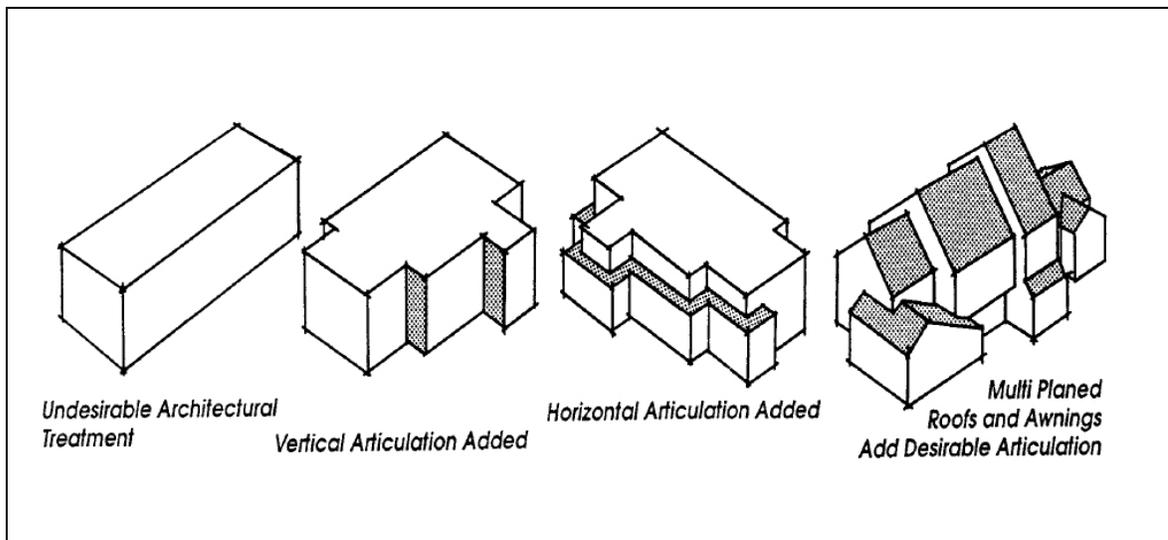
Massing

Intent:

To reduce the impact of the large bulk of the building.

Standards:

1. Break down the building mass by the following techniques for street facing façades more than 100 ft long shall be treated with the following:
 - a) Change in the roof or wall plane (4 ft minimum at every 100 ft). This does not have to be in a regular interval, if specific design is approved by the Planning Director.
 - b) Use projecting or recessed elements in the façade.
 - c) Vary the building rooflines.



Façade Treatment

Intent:

To break the monotony of continuous building façades.

Standards:

- 1. Modulate and articulate the façade.
- 2. Treat the solid wall with landscaping, trellises, canopies, inserted lighting, modulation and articulation.
- 3. Provide more window/ visibility/ indoor-outdoor relationships.
- 4. Select color and material to add variety.



Fred Meyer renovation
Bellingham, WA

ARCHITECT
Mulvanny



Albertsons Grocery Store
Bremerton, WA

ARCHITECT
Freiheit & Ho Architects



MIXED-USE DEVELOPMENT STANDARDS

Standards for Mixed-Use Development

Objectives and Applicability

Standards in this section are intended to guide horizontal and vertical mixed-use developments. In Kennewick, mixed-use developments are allowed in all commercial districts except for Commercial, Marina (CM).

All applicable commercial and multi-family design standards (KMC 18.12, KMC 18.75, KMC 18.78,) shall apply to mixed-use developments. The standards of the underlying zoning districts shall also apply. In case of a conflict between KMC zoning district standards and Design Standards, the Design Standards shall take precedence. The Director of Community Planning shall have the final administrative authority to interpret this section.

Overall Intent

To offer specific guidelines for mixed uses in commercial districts. The intent is also to encourage more efficient use of land and public services, increase economic return, reduce transportation costs, encourage human interaction, create place-making with day and evening activities, create a more sustainable development pattern, and increase development opportunities.

General Development Standards

Mandatory:

Density (units/ acre) and Lot Coverage

1. Minimum density of 13.1 units per acre and maximum density of 27 dwelling units per acre. (See High Density Residential and Medium Density Residential standards for density calculations, Section 18.12.010)).
2. Lot coverage is limited to a minimum of 60% for a single building mixed-use project and a minimum of 50% for a multiple building/multiple site mixed-use project and a maximum of 80% for both types of mixed-used projects.

Height Limitation

Mandatory:

1. Height shall be in accordance with KMC 18.12 for each zoning district. If residential uses are on the ground floor (horizontal mixed use scenario) of a newly constructed building/project, the minimum height for residential buildings shall be 25 feet and the space shall be built to allow for potential future conversion to non-residential uses.

Multiple Building / Multiple Site Mixed-Use Development Projects

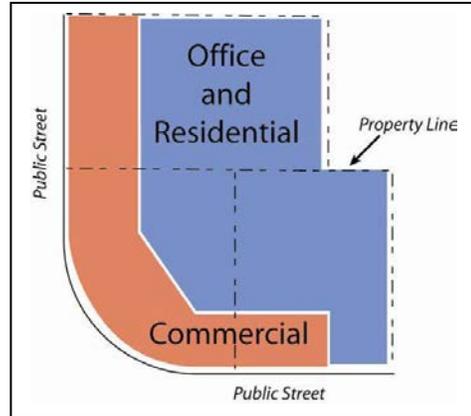
(These standards are in addition to the mandatory site design standards listed above)

Mandatory:

1. **Ground floor requirement.** At least 50% of lineal street front of the ground floor or a portion as identified in parking standards 3.d below of a mixed use development must be dedicated to commercial uses that are pedestrian-oriented

MIXED-USE STANDARDS

(e.g., retail, services, etc.) if located on highest and medium pedestrian emphasis streets as identified in the Commercial Design Standards or identified by the City. Parking garages can be included in this count as long as they don't consist of more than 25% the site or street frontage. These standards shall also be applicable for arterial and collector street frontages when they are the main street frontages.



2. **Mix of uses.** At least 20% of the combined gross floor area of the buildings proposed on the site shall be devoted to residential uses, and at least 20% to non-residential uses. Parking garages or portions of garages devoted to residential or non-residential uses may be counted towards this requirement, but surface parking may not.



Street frontage retail and commercial use with residential condominiums above.

Luxury townhomes

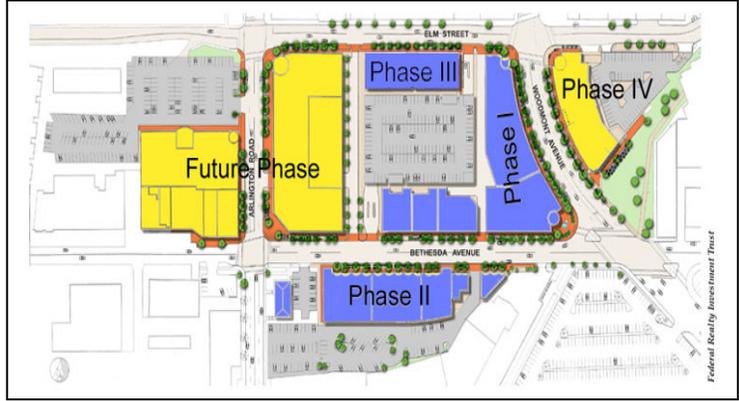
source:

http://www.navenewell.com/n_mixed.php

3. **Functional integration of uses.** Land uses in mixed use sites shall be selected and designed to ensure interaction among uses. Examples include but are not limited to commercial developments which are significantly used for shopping by on-site or adjacent residents or office workers.
4. **Physical integration of uses.**
 - a) All buildings and improvements on the site shall be located and designed to look and function as an integrated development and to encourage pedestrian travel between buildings and uses. Complete visual and physical segregation of use types, such as placement of multiple commercial structures all on one

side of a mixed use site and multiple residential structures all on the other, shall be prohibited.

- b) Separate buildings shall be connected through pedestrian linkages delineated through landscaping, differentiated surface materials or texture. Delineation through striping alone shall not be considered sufficient.
- c) One or more similar design characteristics among separate structures shall be provided, including but not limited to similar or complimentary building façades, massing, surface materials, colors, and/or signage.



Site Design Standards

Mandatory:

1. Multi-Family Design Standards (KMC 18.75) shall apply to all residential portions of horizontal mixed-use developments. If there is a conflict between the Multi-Family Design Standards and the Mixed-Use section of the Commercial Design Standards, the Mixed-Use section of the Commercial Design Standards shall take precedence. All other applicable standards shall apply.
2. **Required open space.** New developments with a lot area of more than 20,000 ft² shall provide a pedestrian-oriented space (plaza, courtyard or similar space) of at least 1% of the site area as described below. Buildings that front on a public sidewalk and have a pedestrian-oriented commercial use facing that sidewalk are exempt from this open space requirement.

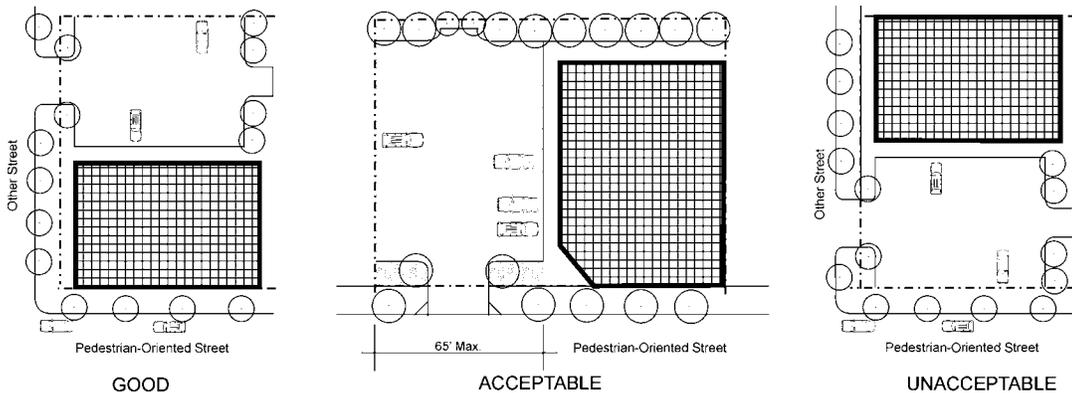


- Outdoor pedestrian-oriented space must include:
 - Visual and pedestrian access (including barrier-free access) to the abutting structures from the public right-of-way or a non-vehicular courtyard.
 - Paved walking surfaces of either concrete or approved unit paving, in areas intended for foot traffic.
 - On-site or building mounted lighting provided at least 0.6 foot-candles (average) on the ground.
 - At least 3 feet of seating area (bench, ledge, etc.) or one individual seat per 60 square feet of plaza area or open space.
 - Landscaping to enhance the area.

- At least 30% of the plaza or courtyard must be shaded by structure or tree canopy (at 10 years after planting).
- Outdoor pedestrian-oriented space shall not include:
 - Asphalt or gravel pavement.
 - Adjacent unscreened parking lots.
 - Adjacent chain link fences.
 - Adjacent “blank walls.”
 - Adjacent dumpsters or service areas.
 - Outdoor storage or retail sales (shopping carts, potting soil bags, firewood, etc.) that do not contribute to the pedestrian environment.

3. Parking.

- a. Residential and commercial parking shall be physically separated or clearly designated. Parking shall comply with the Commercial and Residential Design Standards
- b. Vehicular access to commercial and residential parking areas and garages shall be from secondary streets. When there is no secondary streets available, vehicular access can be located from the available street frontage.
- c. Parking lots may not occupy the corner directly facing a street intersection.
- d. On highest and medium pedestrian emphasis streets, parking shall not be located between the street and the primary building. If the City finds that there is no other workable option, the City may allow an individual building or development to have parking on no more than 65’ of the street frontage, or no more than 50% of the street frontage (whichever is greater). Parking garages can be included in the count as long as they don’t consist of more than 25% of the site or street frontage. These standards shall also be applicable for arterial and collector street frontages when they are the main street frontages.



- e. Portions of site frontages that are devoted to off-street parking or access shall include design features such as landscaping so as to maintain visual continuity of the street and sidewalk to the fullest extent possible.
- f. Structure parking – secondary access for individual garages; access to garages from minor street
- g. Reduction in required parking for structured parking.

Possible Parking Reductions

- The number of parking required can be lowered by 15% for parking areas greater than 16,000 sq-ft (KMC 18.72.080).
- If adjoining facilities can be developed and designed efficiently and economically as one facility, and the total parking area will be 10,000 square feet or more the total combined required parking stalls may be reduced by 10 percent (KMC 18.72.090(3)).
- The required number of parking can also be reduced by 10% if the developer includes an improved, permanent on-site bus stop, provided the bus stop is on a transit route, and development is in accordance with the Benton Franklin Transit Authority (KMC 18.72.090 (4)).
- While business can reduce the total number of the parking stalls, they are allowed to preserve some unpaved areas for overflow parking. Such areas can be treated with grasscrete, gravel, or landscaping with a combination of artwork, plazas, or playgrounds that later could be used for parking during the busiest times of the season.
- Parking lots larger than the minimum required in the Code should have a 10% increase in the total landscaping area, number of trees, shading and pedestrian walkways in addition to other design standard requirements.

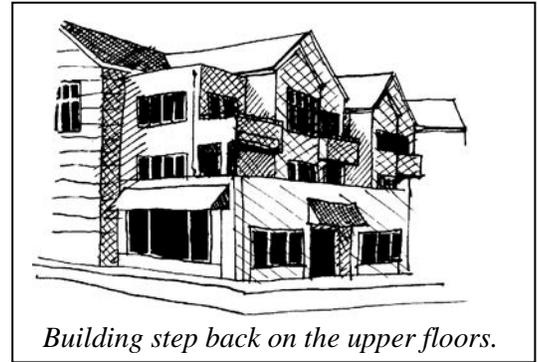
Building Design Standards

Mandatory:

1. Multi-Family Design Standards (KMC 18.75) shall apply to all residential portions of horizontal mixed-use developments. If there is a conflict between the Multi-Family Design Standards and the Mixed-Use section of the Commercial Design Standards, the Mixed-Use section of the Commercial Design Standards shall take precedence.
2. Prominent, visible, illuminated, and safe entrances shall be provided that lead to upper floor residential and ground floor commercial.
3. Ground floor commercial shall comply with the mandatory façade transparency standards.
4. Buildings shall be placed on a site in a manner that encourages pedestrian activity within and along the site.
5. Blank walls longer than 30 feet without windows or an unbroken series of garage doors are not permitted on any street frontage, including frontage to controlled access highways and freeways.

Recommended:

6. Buildings should step back on the upper floors to reduce the bulk.
7. Enhance ground level expression:
 - a) Ground level design should act as a base for the building in terms of massing.
 - b) Ground level commercial should have transparent doors and windows, canopies, plinth, pedestrian scale signage and similar design features.



Wider business front walking corridor at the ground level is encouraged when combined with the street sidewalk. 8. Middle floors between retail commercial on the ground and residential on the top can be used as office (where the zoning permits). This can act as a sound buffer between retail in the ground and residential in the upper floors.